

Vision Zero, Riga – 2 December 2020



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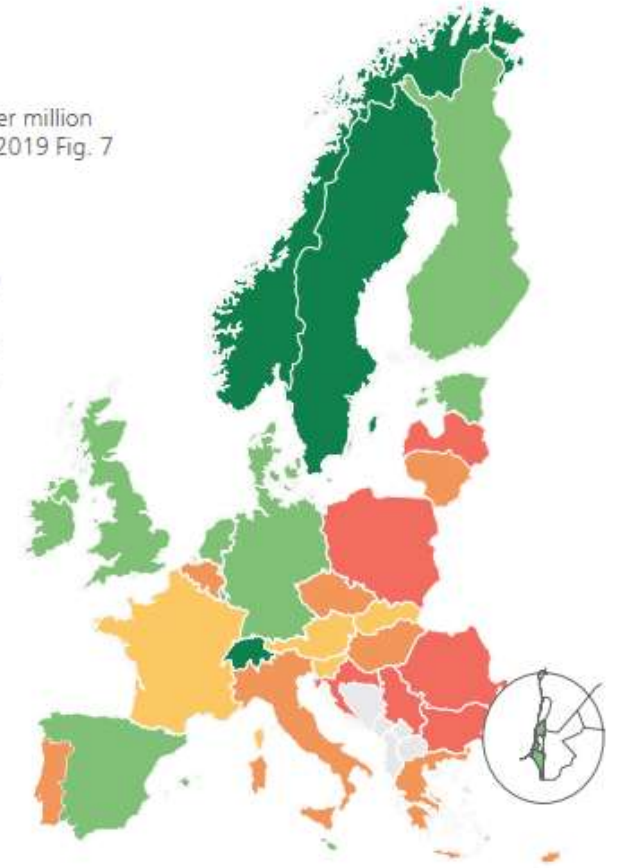
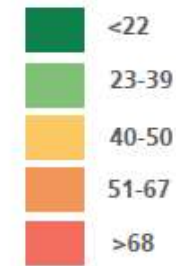
- ✓ **An NGO with a science-based approach to road safety**
- ✓ **Secretariat** in Brussels
- ✓ **50+ member organisations** from across Europe
- ✓ More than **200 experts** contributing to ETSC's work
- ✓ The European Commission, Member Organisations, Member States and corporate sponsors are funding our work

ETSC PIN PROGRAMME

- 32 participating countries, including all EU Member States, and experts in each
- Annual report
- 40 PIN Flash reports
- PIN Talks

MAP 2:

Road deaths per million inhabitants in 2019 Fig. 7



TOYOTA





HOW TO IMPROVE
THE SAFETY OF
GOODS VEHICLES IN THE EU?

PIN Flash Report 39

May 2020

PIN Flash Report 39

How to improve the safety of goods vehicles in the EU

Published May 2020

3310

KILLED

IN COLLISIONS WITH
**HEAVY GOODS
VEHICLES (HGVs)**



2630

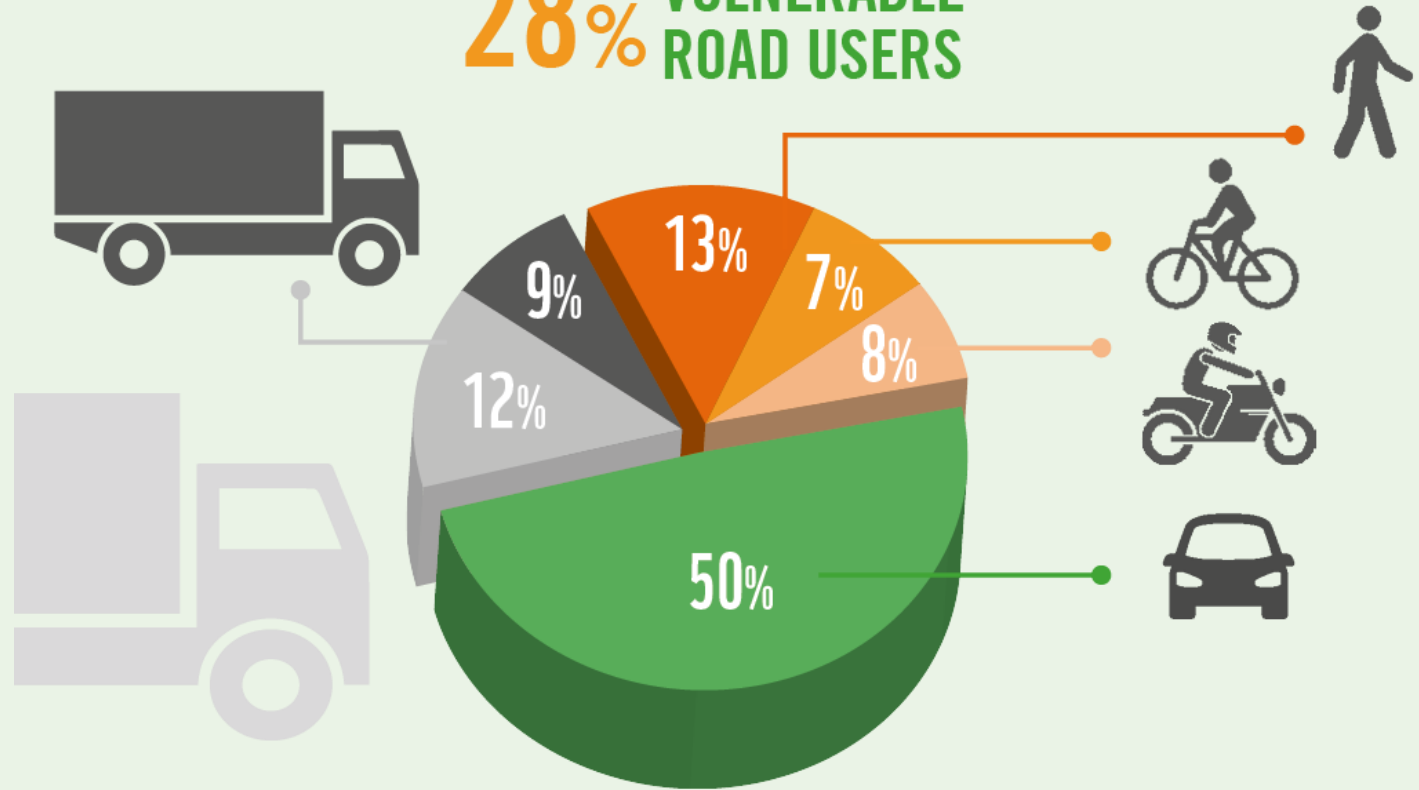
KILLED

IN COLLISIONS WITH
**LIGHT GOODS
VEHICLES (LGVs)**

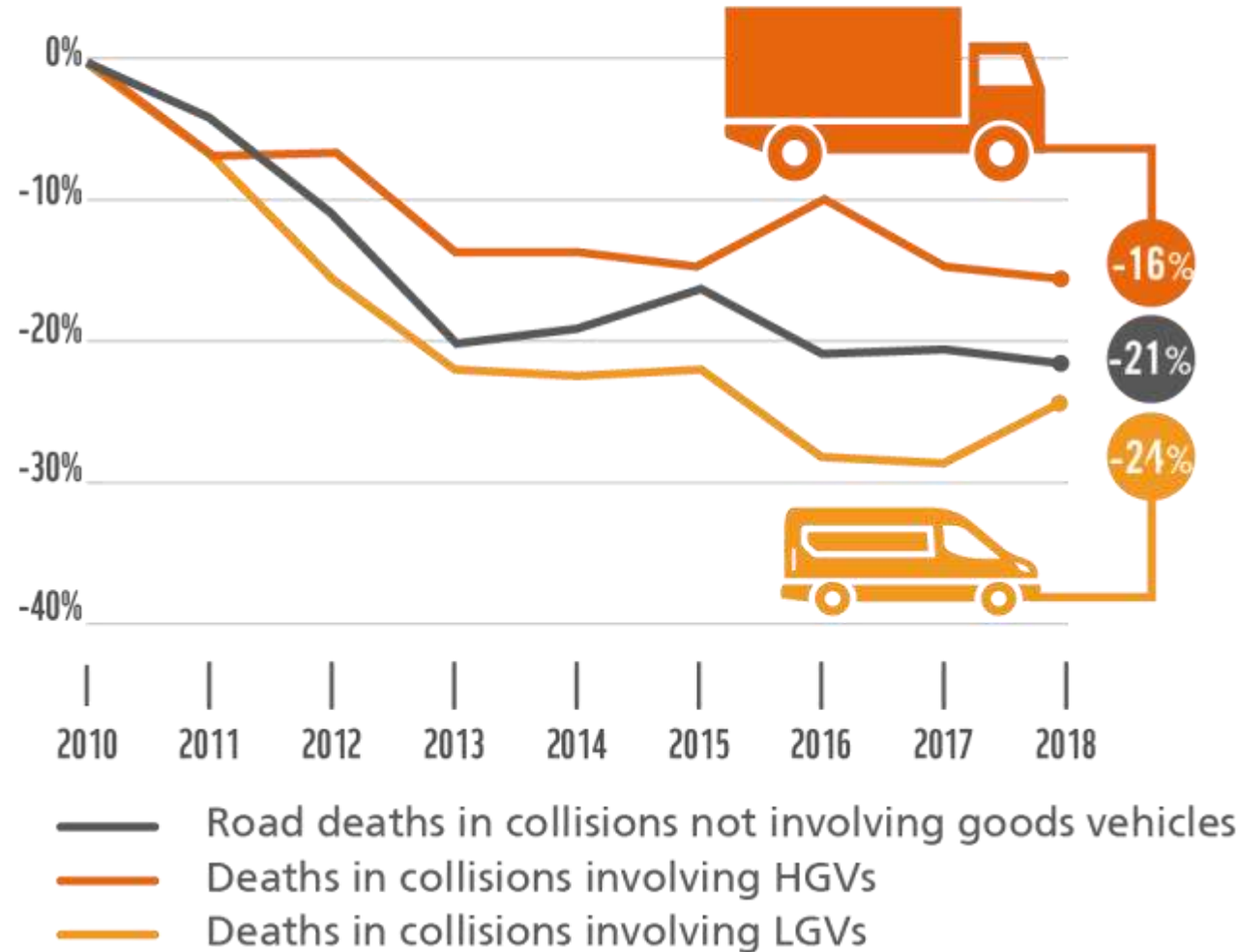


WHO DIES IN COLLISIONS INVOLVING HGVs?

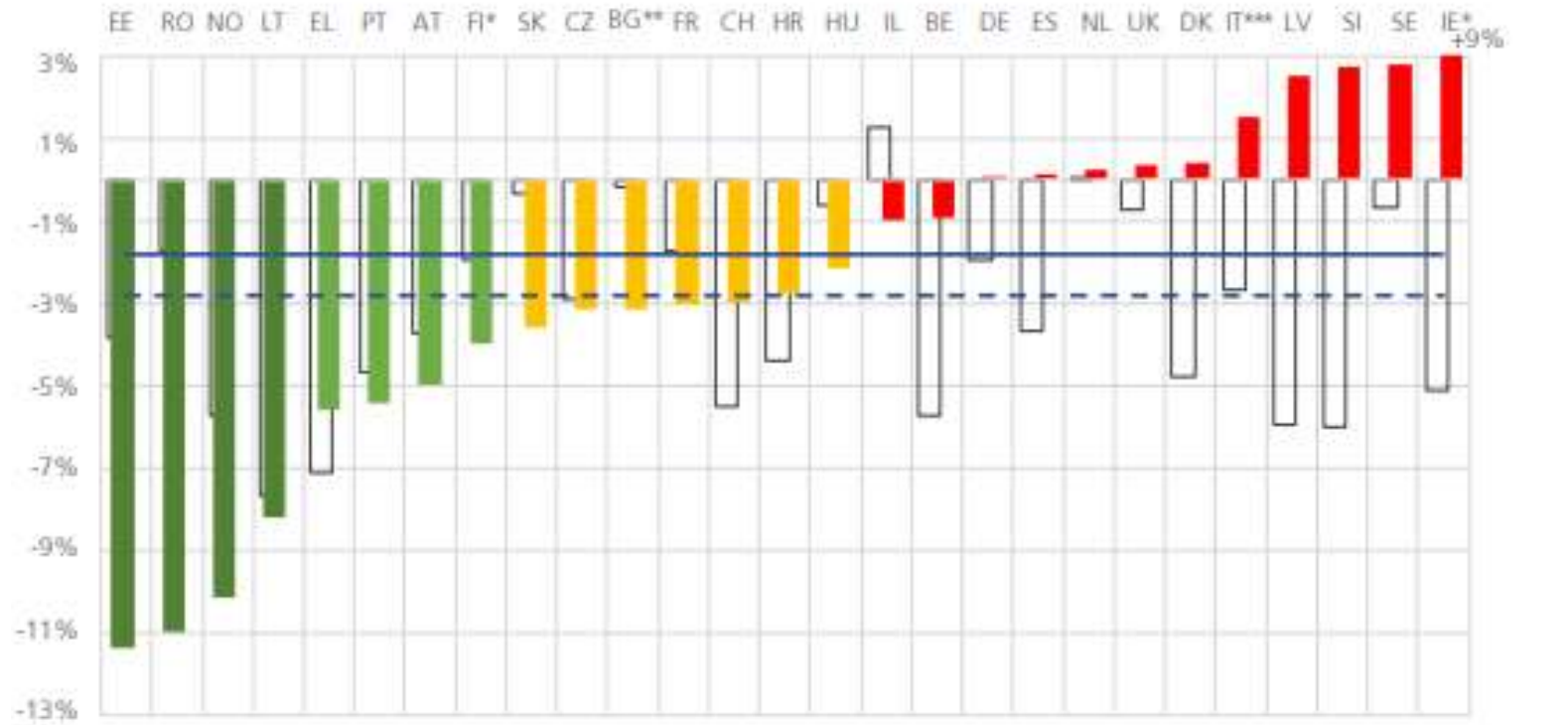
**28% VULNERABLE
ROAD USERS**



DEATHS IN HGV COLLISIONS ARE DECLINING MORE SLOWLY THAN OTHER ROAD DEATHS:

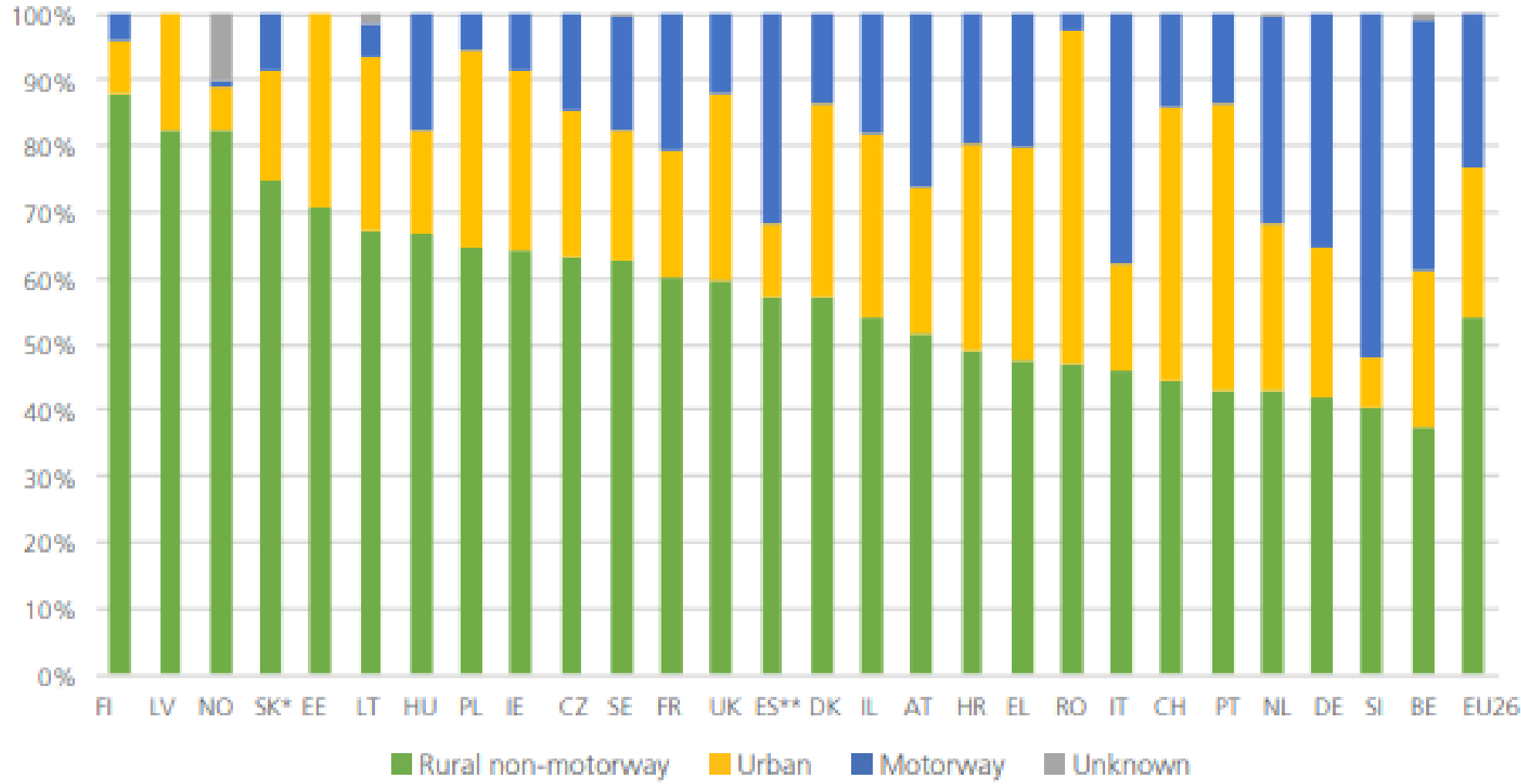


Country progress in reducing deaths in collisions involving HGVs



- Average annual reduction in road deaths involving HGVs
- Average annual reduction of road deaths in collisions not involving goods vehicles
- EU25 average in annual reduction of road deaths involving HGVs: -1.8%
- EU25 average in annual reduction of road deaths in collisions not involving goods vehicles: -2.8%

Proportion of reported road deaths by road type in collisions involving HGVs (average last 3 years)





Observed HGV speeds
above the speed limit
in free-flowing traffic in a
range of countries

17%-64% ABOVE
speed limit

URBAN ROADS

8%-30% ABOVE
speed limit

RURAL ROADS

2630

KILLED

IN COLLISIONS WITH

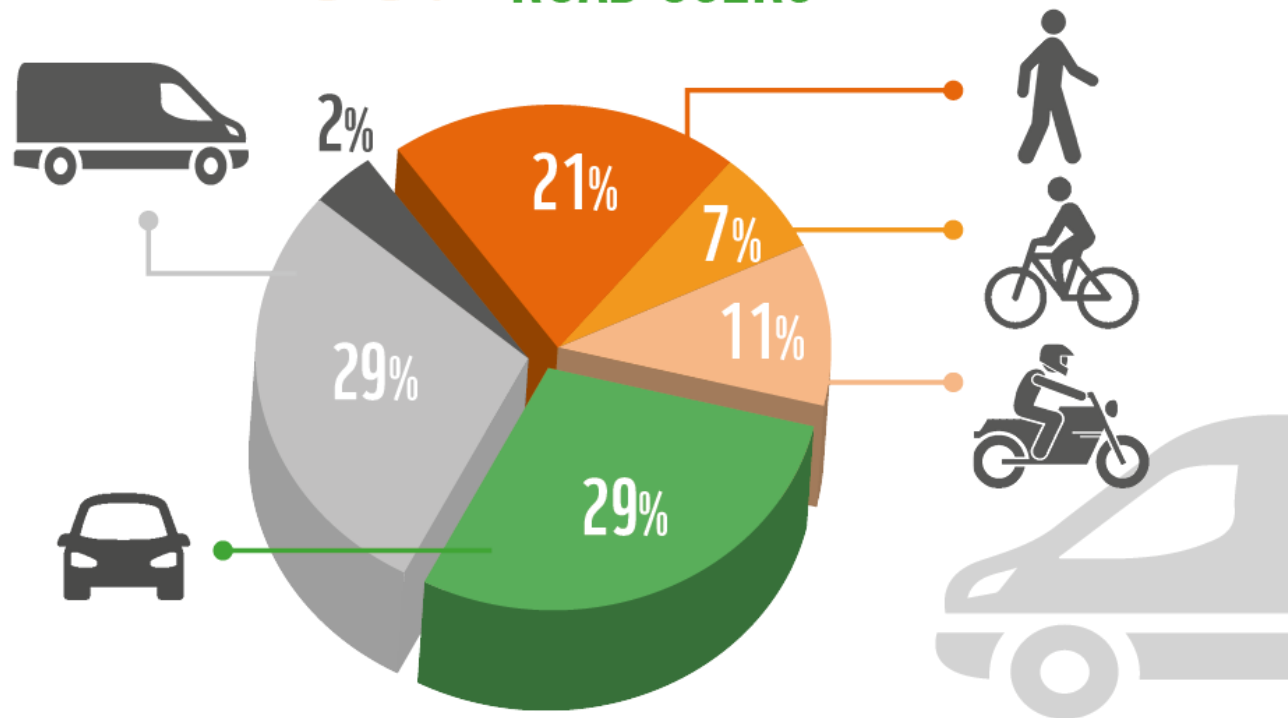
LIGHT GOODS

VEHICLES (LGVs)

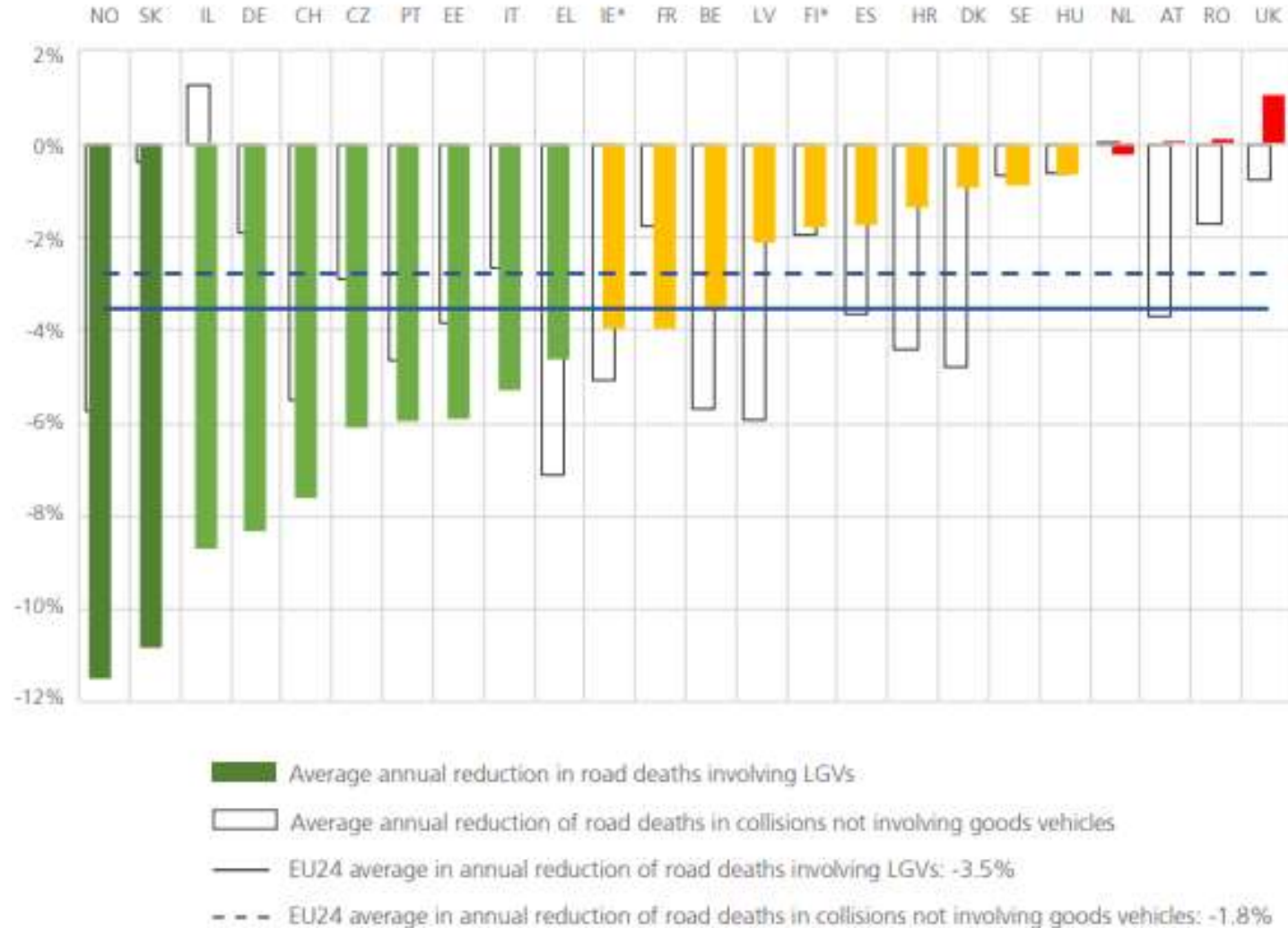


WHO DIES IN COLLISIONS INVOLVING LGVs?

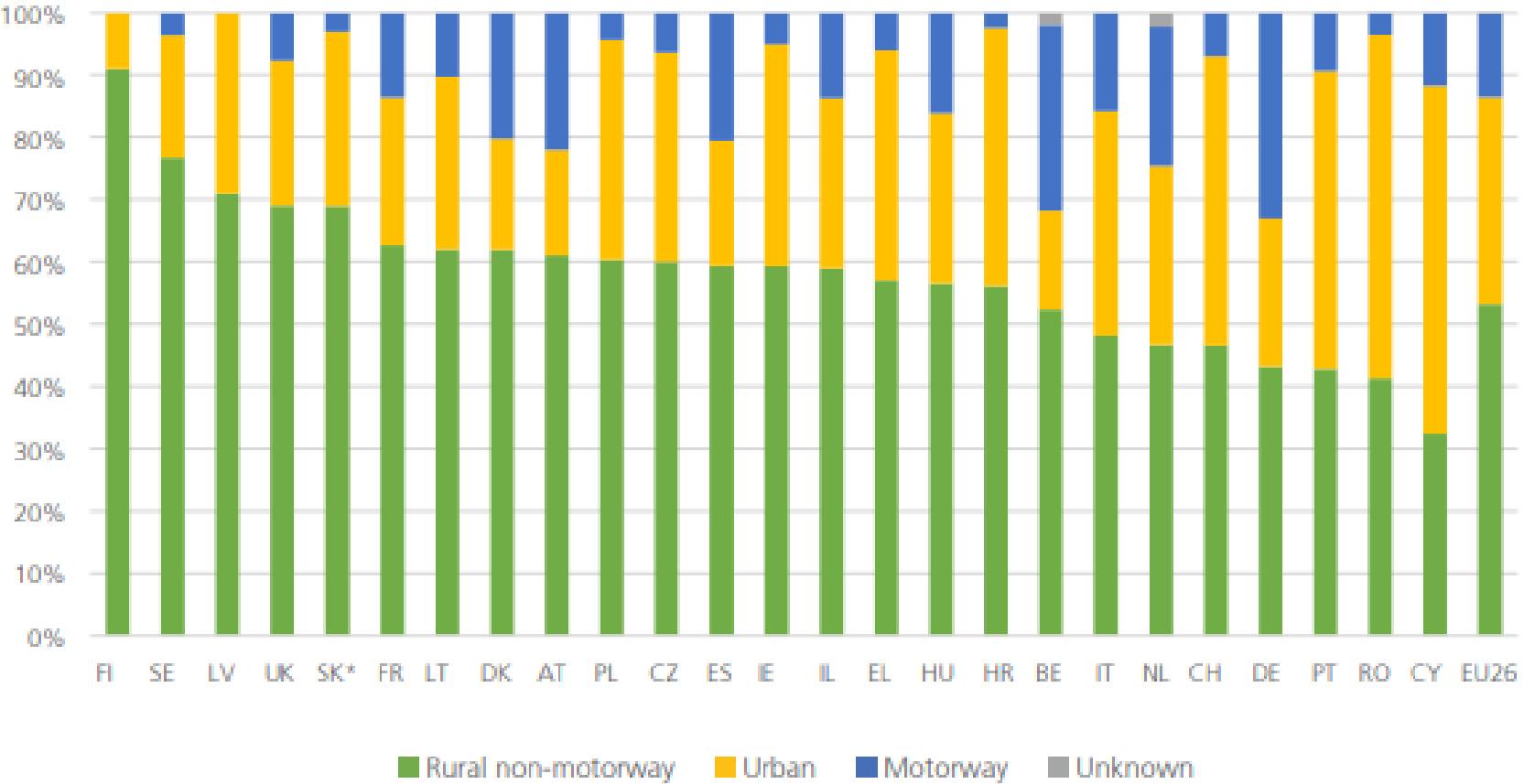
39% **VULNERABLE ROAD USERS**



Country progress in reducing deaths in collisions involving LGVs



Proportion of deaths by road type in collisions involving LGVs (average last 3 years)



- **Vehicle safety** - EU's General Safety Regulation and Pedestrian Safety Regulation updated in 2019
- **Driving and resting hours** – extend to cover all drivers operating LGVs for commercial purposes, not just those engaged in international transport.
- **Professional training** - important tool for reducing work-related road risk.
- **Distraction** – shown to be a risk but we need data!
- **Fatigue** - factor in approximately 20% of commercial road transport collisions.
- **Seatbelts** - Drivers of HGVs and LGVs tend to show lower seatbelt usage rates compared to car drivers
- **Alcohol and drugs** - Driving under the influence is less prevalent in goods and services transport compared to private transport

RECOMMENDATIONS



**Better direct
vision for HGVs**



**Good performance
of Intelligent Speed
Assistance capable of
detecting stricter speed
limits for HGVs**



**Traffic law enforcement
for HGVs and LGVs,
including speed
compliance**



**Public procurement
of safe goods
vehicles**



**Regulation on LGV
working hours
and rest periods**



**Require professional
driver training for
LGV drivers**

TO MEMBER STATES

- Collect data -collisions involving HGVs and LGVs, on speed compliance, average speeds and travel data.
- Include safety as a criterion for public procurement involving the use of goods vehicles
- Enforce compliance with speed limits
- Develop and implement national enforcement strategies to target speeding, intoxicated, dangerous and distracted driving and non-use of seatbelt by goods vehicle drivers.
- When possible, separate traffic in opposite directions by a median barrier and install side barriers.

TO THE EUROPEAN UNION

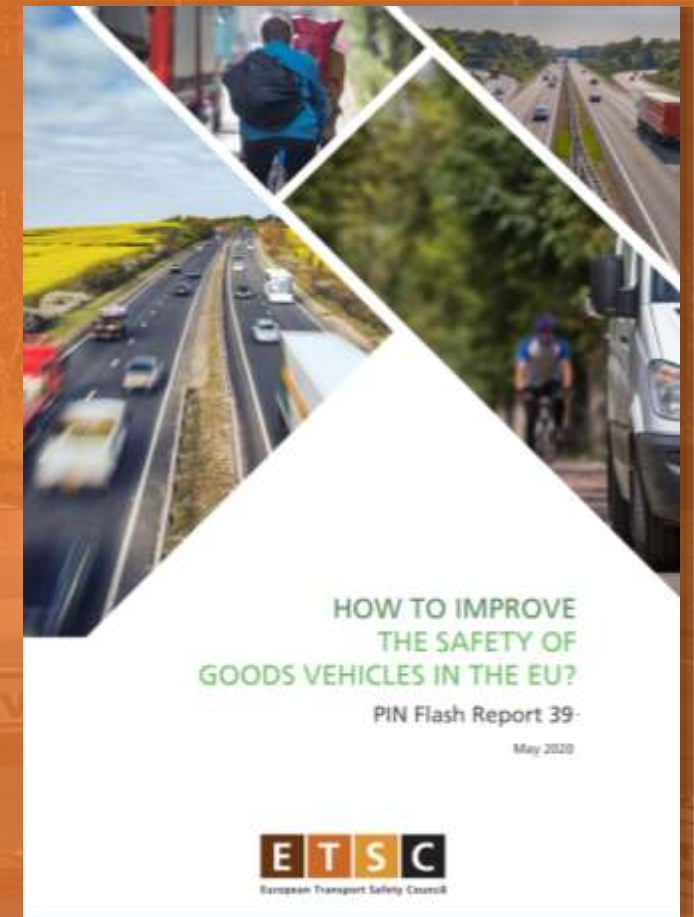
General Safety Regulation (GSR)

- Deliver on the estimated number of deaths and serious injuries prevented
- Insist on the highest achievable vehicle regulation standards at UNECE with regards to blind spot detection systems and direct vision;
- Require a high level of performance of Intelligent Speed Assistance (ISA) systems to be fitted in all new vehicles;

TO THE EUROPEAN UNION

- Extend the legislative framework for working time and driving and resting hours to cover all professional LGV drivers, not just international transport.
- Extend the current CPC requirement (professional driver training) to all professional LGV drivers
- Develop multidisciplinary and holistic strategies to educate, instruct, train and enable employers to better manage commercial vehicle risk management practices in the workplace and on the road.

THANK YOU!



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